

Addendum No. 1

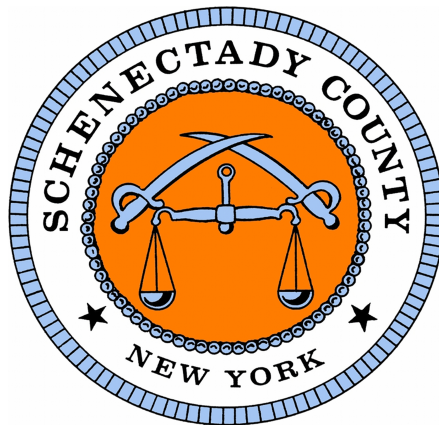
For

FLEXPOD HANGAR DEVELOPMENT

Schenectady County Airport

Contract No. RFB-2021-47
NYS DOT PIN No. 1905.04
CHA Project No. 054338.000

December 7th, 2021



TO ALL BIDDERS:

The following information constitutes **Addendum No. 1** of the Contract Documents. Each bidder shall acknowledge receipt of this Addendum.

All revisions to the Drawings and Specifications called for in this Addendum shall be reflected in the Construction Drawings and Specifications issued to the successful bidder at the Pre-Construction Meeting.

The following addendum items modify, change, delete from or add to the requirements of the contract documents for this project. The articles contained in the addendum take precedence over the requirements of the previously published contract documents. Where any article of the contract specifications or any detail of the contract drawings is modified or any paragraph, subparagraph or clause thereof is modified or deleted by the articles contained in this addendum, the unaltered provisions of that articles, paragraph, subparagraph or clause shall remain in effect.

ITEM No. 1 – CLARIFICATIONS

1. Apprenticeship Program Certification

Should the contract be over \$200,000, the Prime contractor must certify that they currently have a New York State Certified Apprenticeship program, and the Prime contractor must certify that all subcontractors also have a currently certified New York State Apprenticeship program.

2. M/WBE Goal and Workforce Utilization

While this is an Airport Project and usually FAA projects require a DBE component, it is being fully funded by a NYSDOT Aviation Bureau Grant, which requires the Prime Contractor to meet both an M/WBE Goal (Subcontractors and Materials), and also a M/WBE Workforce Utilization goal. The contractor is directed to the Bid Documents to locate the required percentages for each of these project components.

3. Project Calendar Days

*The contractor shall be allowed a total of **120 Calendar Days** to complete this project. The Calendar Days will start within 10 Days of the Notice to Proceed date and/or the first day the Contractor is on-site (whichever happens first). The contractor shall make note that the Calendar Days noted on the **Construction Safety and Phasing Plan (CSP-1)** are to be 120 and not 90 as currently shown on the Bid Plans. This update will be made and provided as part of the “Issued for Bid” plans.*

4. County Contract Number Update

*The contractor shall make note that the County Contract Number for this bid is **RFB-2021-47**. Any reference to the old bid number of **RFB-2021-36** will be corrected. This update will be made and provided as part of the “Issued for Bid” plans.*

5. Pre-Bid Meeting Minutes

Attached as part of this Addendum are the Meeting Minutes for the Pre-Bid Meeting held on December 23rd, 2021.

ITEM No. 2 – CONTRACTOR'S QUESTIONS

1. Is it possible the Owner would consider accepting a “Blanket Policy” and being named as a “Loss Payee”, as opposed to the “Builders Risk” policy with the owner as additional insured as defined in the Bid Documents?

Yes – Schenectady County is accepting of both the General and Electrical awarded Contractors to provide a Blanket Policy with Schenectady County being listed as a “Loss Payee”.

The General and Electric Contractors should also be aware that when providing insurances, they are required to provide them to cover the entire cost of the project, and not only their individual contract. The cost of the total project will be provided to each Contractor prior to Award of Contract, at which time they will then be required to issue insurance documents for the total project value.

2. With the project being funded by NYSDOT, will only the State Wage Rates be required, or will Federal Wage Rates also be required?

Only the New York State Wage Rates will be required under this project. The contractor is expected and required to monitor these Wage Rates to use the most current during the duration of construction.

3. Are there Liquidated Damages associated with this project?

Yes, the project will contain liquidated damages of \$2,000 per day, when construction goes past 120 Calendar Days. The contractor should be aware though that a "clock stoppage" may be approved (by Schenectady County) as justified by Contractor, should an issue with material lead time and/or inclement weather allow.

4. The plans appear to be designed based on an "Erect-A-Tube" product. Can an alternative manufacturer be utilized as long as it meets the requirements and is cost effect?

Yes, while the plans are based on an "Erect-A-Tube" design, the contractor is not REQUIRED to utilize Erect-A-Tube. The contractor is although required to utilize and submit a product that meets or exceeds the same requirements and standards of the "Erect-A-Tube" design. All material submissions, regardless of manufacturer, will require review and approval by the Engineer.

Any components of the "Erect-A-Tube" design that are proprietary to them will not be required to be met.

5. The Base Bid includes the new Asphalt Apron, yet the plans show the asphalt rebate on the Taxiway side (milling the edge for tie-in to existing pavement) in Alternate 2. Is this just a typo?

*Yes – The reference to the Asphalt Rebate work adjacent to the existing apron as shown on Sheet D-1; **Demolition Plan** is incorrect and should be stated as a requirement under the Base Bid.*

6. Is the Electric Contract responsible for asphalt removal and patching the asphalt for his or her Electrical ditches?

The General or Prime Contractor shall be required to complete the work associated with the electrical installation within existing pavement. This effort includes Saw-cutting the existing pavement, unclassified excavation of the trench, backfilling with acceptable material, and placement of new P-401 Hot Mix Asphalt. The work and materials as described above shall be considered incidental to the project, and shall not be quantified individually under the like contract items.

7. Will flagging be required on equipment if working on the outside of the Temporary Security Fence?

Flagging on Construction Equipment is not required for equipment and vehicles shorter than 20 feet in height. For any items that extend over 20 feet in height (i.e., - Concrete Boom Truck, Crane, etc), they will be required to be flagged.

Additionally, a 7460-1; Notice of Proposed Alteration or Construction for both Temporary and Permanent items have been submitted to the FAA for review. They have not provided a determination as of the current date, but we have proposed a maximum Crane Boom height of 60 feet. Should this height be approved, the Contractor will be required to follow any guidelines required by FAA under the approval determination (i.e. – Flagging Crane Boom and/or Obstruction Lighting on Crane Boom).

Should the Contractor be required to utilize any equipment more than 60 feet in height, they will be required to submit a new 7460-1; Notice of Proposed Alteration or Construction for approval.

8. **On CSP-2, the Detail for Temporary Fencing on Concrete Barrier only apply to where it sits on existing pavement? Can the side lot posts be set in soil?**

Contractor shall be permitted to utilize Concrete Barrier fencing Only in the areas that it is placed on existing pavement. In locations where the fence is to be installed above grass, the contractor shall be allowed to drive posts (4' minimum depth) to complete the installation. Contractor would be expected to locate any underground utilities prior to driving posts should they go that route at their own costs. Any damage to underground utilities due to this work shall be repaired immediately and at not cost to the Sponsor.

9. **Who will be responsible for the electric utility charges for new service?**

The Electrical Contractor is required to coordinate the Electrical Utility costs associated with installation of the New Service directly with National Grid, and is also required to pay for the cost of this work completed by National Grid per Specification Section 011200, Section 1.7B.

ITEM No. 3 – FRONT ENDS

Bid Schedule/Tabulation Form

The contractor shall be advised to remove the Bid Schedule Form (Pages BS-1 through BS-21) from their official bid submission, it is not required.

The successful bidder shall note that upon award of this contract, they will be asked to provide a Unit Cost for items found in the Contract Plans under the "Items Quantities" table for this project. These unit costs will be utilized for payment on this contract.

ITEM No. 4 – SPECIFICATIONS

None



PRE-BID MEETING AGENDA

Schenectady County Airport
FLEXPOD HANGAR DEVELOPMENT
County Contract No. RFB-2021-47
NYSDOT PIN 1905.04

Date: November 23rd, 2021
Time: 11:00 am
Location: Schenectady County Airport – Richmor Aviation – Training Room / Site-Walk

Attendees: See Sign-In Sheet

OWNER / SPONSOR:

- Schenectady County (Department of Aviation)
 - Joe Landry – Commissioner of Aviation
 - Peter Knutson – Senior Civil Engineer
 - Michael Schadewald – Airport Operations Manager

CONSULTING ENGINEER(S):

- CHA Consulting, Inc.
 - Andrew Pappalardo – Project Manager
 - Todd Ewell – Senior Project Engineer
 - Kristin Dawe – Senior Design Engineer
 - Matthew Florell – Project Engineer / Senior Inspector

CONTRACT / BID DOCUMENTS:

- Obtaining Plans and Specifications:
 - BidNet Direct (www.bidnetdirect.com/new-york)
 - Formal Request to *Schenectady County Purchasing Department* (purchasing@schenectadycounty.com)
 - Printed Plans and Specifications requested through *Schenectady County Purchasing Department* (requires 24 hour advance notice)

- Bid Opening:
 - All Bids must be delivered to the *Director of Purchasing*, No Later than **December 9th, 2021 at 1:00pm**
 - Bids delivered after 1:00am will **NOT** be accepted
 - Bids will be opened on **December 9th, 2021 at 1:00pm** at:
Schenectady County Purchasing Department
County Office Building
620 State Street
Schenectady, New York 12305

BIDDING REQUIREMENTS:

- Acknowledgement of Addendums
 - A minimum of (1) Addendum will be issued prior to Bid Opening
 - Addendums will be disseminated by *Schenectady County Purchasing Department* to all documented plan holders
 - All Bidders must acknowledge receipt of issued addendum on Page 2 of the Bid Form
- Bid Security Check
 - Certified Check, Payable to **County of Schenectady** for **5%** of total Bid amount
 - Security Check will be returned to unsuccessful Bidders once project is awarded
- Minority/Women Business Enterprises (M/WBE)) Goals and Commitment
 - NYS Funded projects require M/WBE percentages to be met.
 - 10.0% MBE Goal
 - 15% WBE Goal
- Workforce Utilization Goals and Commitment
 - NYS Funded projects require M/WBE Workforce participation percentages to be met
 - 3.2% Minority Workforce Utilization Goal
 - 6.9% Female Workforce Utilization Goal

CONTRACT AWARD:

- Construction Funding
 - NYSDOT Aviation Capital Grant Program
 - Grant Issued January 2019
- Award of Contract
 - Contract Awarded to Low Bidder meeting all requirements
 - Responsive and Responsible
 - Providing Performance Bond (100% of Bid Amount)
 - Providing Labor Bond (100% of Bid Amount)
 - Meets M/WBE Goals (or accepted Good Faith Effort; approved by County Purchasing Agent)
 - Apprenticeship Program Certification
 - Bidders are expected to fully review the *General Instructions to Bidders* and all other Bid Documents for full list of requirements.
- Notice to Proceed
 - NTP Issued to Low Bidder upon full execution of Contract with Schenectady County
 - NTP Expected for Spring 2022 (Possibly Earlier)
 - Contractor shall start construction within **10 Days** of NTP

PROJECT DESCRIPTION:

- This project includes the construction of a new 4-Unit Flexpod Hangar, Asphalt Aircraft Apron, Tenant Parking, Drainage Improvements, and Stormwater Improvements. The proposed Flexpod Hangar will have (4) 2,987 SF Hangar Bays for a total of 11,948 SF of Hangar Space.
- The project includes two individual ADD Alternatives to the Base Bid:
 - ADD Alternative No. 1 includes the construction of the 4th FlexPod Hangar Unit and all associated foundation, structure, and electrical components.
 - ADD Alternative No. 2 includes the construction of a landside asphalt parking lot, which includes HMA paving and pavement markings.

DURATION AND PHASING:

- BASE BID
 - Includes a single Work Area
 - 120 Calendar Days
- ADD ALT No. 1
 - Included in the Base Bid Work Area
 - Runs concurrent to the Base Bid 120 Calendar Day duration
- ADD ALT No. 2
 - Included in the Base Bid Work Area
 - Runs concurrent to the Base Bid 120 Calendar Day duration

UNCLASSIFIED EXCAVATION:

- While not expected, Shale is prominent around the airport, and Contractor shall be aware that it may be encountered within excavation limits.
- Previously encountered Shale has always been removable with standard excavation equipment.
- Contractor is expected to review Soil Borings.

P-401 ASPHALT PAVEMENT:

- P-401 FAA Mix Asphalt
- Payment based on Percentage Within Specification Limits (PWL)
 - FAA General Provisions – Section 110
 - Contractor expected to review prior to construction
 - Engineers Testing Firm responsible for QA at Plant, will calculate PWL with Contractor tester
- Contractor responsible for QC Testing (At Plant and In-Field)

OPEN DISCUSSION:

